



Town of Webster
Highway Department

Driveway/Curb Cut Regulations

A. CURB CUT PERMIT

1. A Curb Cut Permit shall be required for the new construction or alteration of any regularly used access to a public way. Any abutting property owner desiring to gain access to a way shall do so only in accordance with the provisions of a permit issued by the Town of Webster Highway Department.
2. Application Procedure: Before beginning construction, the abutting property owner shall make written application to the Highway Department, including:
 - a. Permit fee's New Construction \$50 Addendum \$25
 - b. A plan drawn to scale showing:
 - i. The boundaries of the abutting property and any structures existing or proposed thereon;
 - ii. The location of any driveway that is to be newly constructed or altered, including the location of the turnaround;
 - iii. The grading of the proposed driveway and any additional means of stormwater drainage (swales, culverts, catch basins, etc.)
3. Inspections: All work shall be inspected by the Highway Department during and after construction. The abutting property owner or their agent will notify the Highway Department of the date and time of construction at least 24 hours before construction begins. The Highway Department may halt any work not done in accordance with the permit.

B. DESIGN REQUIREMENTS

1. The Highway Department shall consider the requirements of the Mass Highway Manual on Uniform Traffic Control Devices and Highway Design Manual, but shall modify these to accord with:
 - a. Local conditions;
 - b. Compatibility with local road design; c) Size of the proposed project.
2. Driveways shall be at least twelve (12) feet in width and should be located to the best advantage with regard to alignment with the way, profile, sight distance conditions, and the like. Unless conditions require it, a driveway should not be located at the extreme edge of a property, or within ten (10) feet of an existing structure or utility. In no instance shall the driveway intersect the way at less than a sixty degree (60°) angle. In no instance shall a driveway exceed twelve percent (12%) grade, except with the written approval of the Highway Superintendent.
3. Driveways shall be no more than twenty-four (24) feet in width, unless approved by the Town Engineer.
4. No more than two driveways shall normally be allowed for any one property unless there is clear necessity for more, as determined by the Highway Superintendent. Leasing of a portion of the property does not affect this requirement. If a number of establishments will be constructed on one parcel, a service road may be required.
5. Driveways shall not normally be approved within forty (40) feet- per Subdivision Rules & Regulations of intersections, particularly signalized intersections, because of the potential safety hazard that arises when a driver enters a road from a corner driveway and is not faced with a direct signal indication. Access directly into a rotary is also discouraged.

6. In no instance shall a driveway have a pitch towards the roadway of greater than one inch per foot unless adequate provisions have been made and approved by the Highway Department for the diversion of driveway surface runoff away from the roadway. The Highway Department may require methods of diversion for driveways having a pitch of less than one inch per foot if the proposed driveway construction is expected to result in an excess accumulation of surface water in the way.
7. All driveways shall access the way on which legal frontage for that lot is established unless otherwise reviewed and approved by the Highway Superintendent.
8. Common driveways **(see Subdivision Rules & Regulations)**
9. Driveways across existing or proposed sidewalks shall be designed and constructed to allow the safe passage of wheelchairs along the sidewalk. Design of such driveways should be in accordance with the accessibility criteria prescribed by the Massachusetts Architectural Access Board and the Americans with Disabilities Act. **(see Subdivision Rules & Regulations).**
10. In the event that an existing guardrail will be removed to provide an opening for a driveway, the abutting property owner shall be responsible for such removal and installation of proper terminal ends. The Contractor hired by the owner to complete any work associated with guardrail shall be experienced in such work and be approved by the DPW.

C. ENFORCEMENT

1. Any person who continues to violate any provision of this Regulation or of any permit issued hereunder after the expiration of ten days following receipt by him of a written notice of violation shall be liable to a penalty not exceeding one hundred (100) dollars for each offense. Each day that such violation continues after said ten-day period shall constitute a separate offense.
3. The Highway Superintendent shall enforce the provisions of this regulation.

L. Driveways

1. All driveway entrances onto streets shall be constructed that vehicles can enter and exit from the lot in question without posing any substantial danger to themselves, pedestrians, or vehicles traveling in abutting streets. Interference with the free and convenient flow of traffic in abutting streets shall be minimized.
2. Driveways shall meet the following standards:
 - a. Driveways shall be located not less than forty (40) feet from the tangent point of the curb radius of any intersection.
 - b. Driveways to corner lots shall gain access from the street of lower classification when a corner lot is bounded by streets of two different classifications.
 - c. In subdivisions containing single and two family dwellings, driveways shall be not more than twenty (20) feet wide at the street line; have a radius of five (5) feet; and be at least ten (10) feet wide outside of the right-of-way line for single lane driveways.
 - d. Driveway locations shall be shown on the Definitive Plan. Curb cuts of adjacent lots shall not be closer than thirty (30) feet unless otherwise approved by the Board.
 - e. The driveway opening shall be at least ten (10) feet from any existing or proposed structures within the street right-of-way, such as transformers, hydrants, drainage facilities, etc.
 - f. Driveway grades within the street right-of-way shall not exceed two percent (2%) and beyond the right-of-way shall not exceed fifteen percent (15%).
 - g. Driveways shall be paved within the right-of-way. All stumps and other yielding material shall be removed and there shall be provided a minimum base course of twelve (12) inches of processed gravel. Within the right-of-way, driveways shall have a minimum of three (3) inches of bituminous concrete pavement with a bottom course of two (2) inches and a top course of one (1) inch.
3. For new lots created in a subdivision after the approval of the Definitive Plan, the driveways shall comply with the standards listed above, and the applicant shall obtain a driveway permit from the Board of Selectmen before gaining access to the subdivision street.

M. Common Driveways

The Planning Board may allow common driveways in subdivisions in accordance with Section 650:43 of the Zoning By-Law (if approved by Town Meeting) in order to allow for a more efficient traffic flow, to reduce traffic hazards from numerous individual driveways, to consolidate access to lots across wetlands into one crossing, and otherwise where in its judgment such an arrangement will be more advantageous to the neighborhood than separate driveways. Where lots front on residential collectors or arterials, the Board may require that lots be served by a common driveway in order to limit possible traffic hazards on such street.

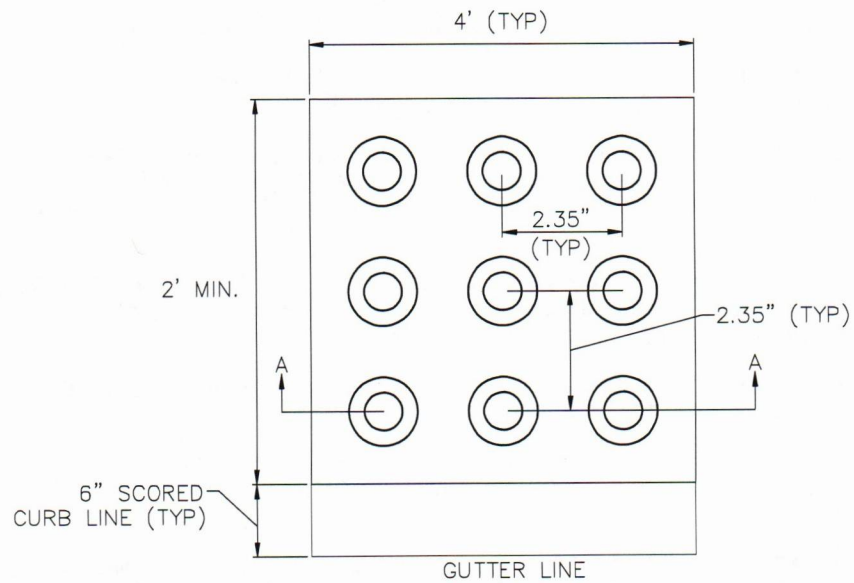
1. Common driveways shall be of sufficient dimensions so as to provide safe travel for vehicles and insure the safety of pedestrians. Where appropriate, the specific standards set forth below may be modified. The Board will give consideration to the number of

Town of Webster, Massachusetts
Subdivision Rules & Regulations

- residences on the common driveway, the character of the neighborhood, and the nature of the terrain over which the driveway passes.
2. Common driveways shall have an easement width of not less than twenty-four (24) feet and shall have a paved width of not less than eighteen (18) feet.
 3. A three (3) foot wide shoulder shall be constructed along each side of the paved or graveled way.
 4. The minimum centerline radius shall be sixty (60) feet.
 5. Grades shall not exceed twelve percent (12%), and shall not exceed three percent (3%) within fifty (50) feet of the street line.
 6. Common driveways shall not exceed five hundred (500) feet in length.
 7. Common driveways shall not be located nearer than sixty-five (65) feet from the intersection of the centerline of any right-of-ways.
 8. Turnaround space shall be provided at the end of the common driveway and shall be capable of serving all vehicles including ambulances, fire engines and police vehicles.
 9. Common driveways shall be completely defined by installing reinforced concrete bounds or iron rods or pipes at each point of change in direction of the easement lines in accordance with Section V.B.12.
 10. The driveway shall be constructed according to the following specifications:
 - a. Organic and non-bearing material shall be removed to at least the full width of the traveled way to provide a lasting and safe driveway; rocks and stones projecting into the subgrade shall be removed to twelve (12) inches below finished grade.
 - b. Driveways shall be brought to subgrade nine (9) inches below finished grade, as shown on an approved plan, with bank gravel compacted to avoid settlement. Water lines, storm drains, culverts and catch basins shall be installed before the finished sub-base is put in place. To the extent feasible, all other utilities shall be installed before the finished sub-base is put in place.
 - c. The finished sub-base of gravel for common driveways shall consist of six (6) inches of clean, compacted gravel containing no stones over two (2) inches in diameter, laid in two (2) lifts of three (3) inches each, and rolled after spreading. Such grading shall be brought to a grade two (2) or three (3) inches below the finished grade shown on the approved plan.
 - d. The driveway, shall have a finished surface of three (3) inches of bituminous concrete with a bottom course of two (2) inches and a top course of one (1) inch.

N. Sidewalks

1. Sidewalks shall be located within the right-of-way separated from the curb face by a shoulder or turf area at least four and one half (4½) feet in width. Sidewalks may vary in horizontal placement to minimize soil disturbance, to save trees, to avoid rock outcrops, etc. If located outside the street right-of-way, an easement eight (8) feet in width shall be conveyed to the Town. Sidewalks shall be five (5) feet wide, have a minimum cross slope of one quarter (¼) inch per foot for proper drainage, and have a maximum grade of eight



DETAIL OF DETECTABLE WARNING PANEL



SECTION A-A

NOTE:
DETECTABLE WARNING PANELS SHALL BE YELLOW.

WHEELCHAIR RAMP NOTES

- 1.) SIDEWALK CROSS SLOPES, AS INDICATED IN THE STANDARD SPECIFICATIONS, WILL BE AT 1.6% MAXIMUM, 1.5% PREFERRED FOR BRICK, CEMENT CONCRETE AND BITUMINOUS CONCRETE. THE ONLY EXCEPTION TO SIDEWALK CROSS SLOPES ON BRIDGES WHICH WILL BE 1%. (REFER TO STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES, SECTION 700). IN ACCORDANCE WITH THE ARCHITECTURAL ACCESS BOARD (AAB) RULES AND REGULATIONS THE SIDEWALK CROSS SLOPE CANNOT EXCEED 2.0%.
- 2.) AN UNOBSTRUCTED PATH OF TRAVEL WITH A MINIMUM WIDTH OF 3' SHALL BE MAINTAINED. THE DESIRABLE MINIMUM WIDTH IS 3'-3".
- 3.) THE WHEELCHAIR RAMP SLOPE AND SIDE SLOPES (TRANSITIONS) MUST NOT EXCEED 1:12. (8.0% MAXIMUM 7.5% PREFERRED) HOWEVER THESE SLOPES MAY BE FLATTER THAN 1:12 WHEN WARRANTED BY SURROUNDING CONDITIONS.
- 4.) HIGH SIDE CURB TRANSITION LENGTH (L_{th}) SHALL BE DETERMINED BASED ON ROADWAY PROFILE GRADE. SEE MASSDOT CONSTRUCTION STANDARD E 107.9.0. (15' MAX)
- 5.) IN NO CASE, WHERE A STOP LINE IS WARRANTED, SHALL A RAMP BE PLACED BEHIND THE STOP LINE.
- 6.) FIXED OBJECTS (i.e. UTILITY POLES, HYDRANTS, ETC.) MUST NOT ENCROACH ON ANY PART OF A WHEELCHAIR RAMP, INCLUDING TRANSITION SLOPES.
- 7.) AT NO TIME IS ANY PART OF THE WHEELCHAIR RAMP, EXCLUDING CURB TRANSITIONS, TO BE LOCATED OUTSIDE OF THE CROSSWALK. WHEELCHAIR RAMP ENTRANCE IS TO BE CENTERED IN THE CROSSWALK WHENEVER POSSIBLE.
- 8.) CATCH BASINS WHICH ARE TO BE LOCATED IN THE VICINITY OF A WHEELCHAIR RAMP SHALL BE LOCATED UP-GRADE OF WHEELCHAIR RAMP ENTRANCE.
- 9.) THE ENTRANCE OF A WHEELCHAIR RAMP SHALL BE FLUSH WITH THE ROADWAY.
- 10.) TESTING SURFACE: WHEN TESTING WITH A STRAIGHTEDGE PLACED PARALLEL TO THE LINE OF SLOPE, THERE SHALL BE NO DEVIATION FROM A TRUE SURFACE IN EXCESS OF 1/4".
- 11.) ALL WHEELCHAIR RAMPS SHALL BE CEMENT CONCRETE.
- 12.) DETECTABLE WARNING PANELS MAY BE PRECAST OR CAST IN PLACE OR OTHER SUITABLE MATERIAL PERMANENTLY APPLIED TO RAMP (SURFACE APPLIED SYSTEMS WILL NOT BE PERMITTED). DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH ADJACENT WALKING SURFACES EITHER LIGHT-ON-DARK, OR DARK-ON-LIGHT. DETECTABLE WARNING PANELS SHALL BE YELLOW. DETECTABLE WARNING PANELS ARE REQUIRED ON ALL PROPOSED WHEELCHAIR RAMPS AND ARE TO BE INSTALLED IN ACCORDANCE WITH MASSDOT CONSTRUCTION STANDARD E 107.6.5 (MARCH 2014).

NOTES:

1. CARE SHALL BE TAKEN TO ASSURE A UNIFORM GRADE ON THE RAMP, FREE OF SAGS AND SHORT GRADE CHANGES.
2. THE BOTTOM OF THE RAMP (GUTTER LINE) SHALL BE BEVELED AND CENTERED ON PAINTED CROSSWALK.

5" SURFACE COURSE CLASS '4000 PSI' 3/4" AGGREGATE CONCRETE COARSE BROOM FINISH TRANSVERSE TO THE SLOPE OF RAMP.

3/8" PREFORMED BITUMINOUS EXPANSION JOINT

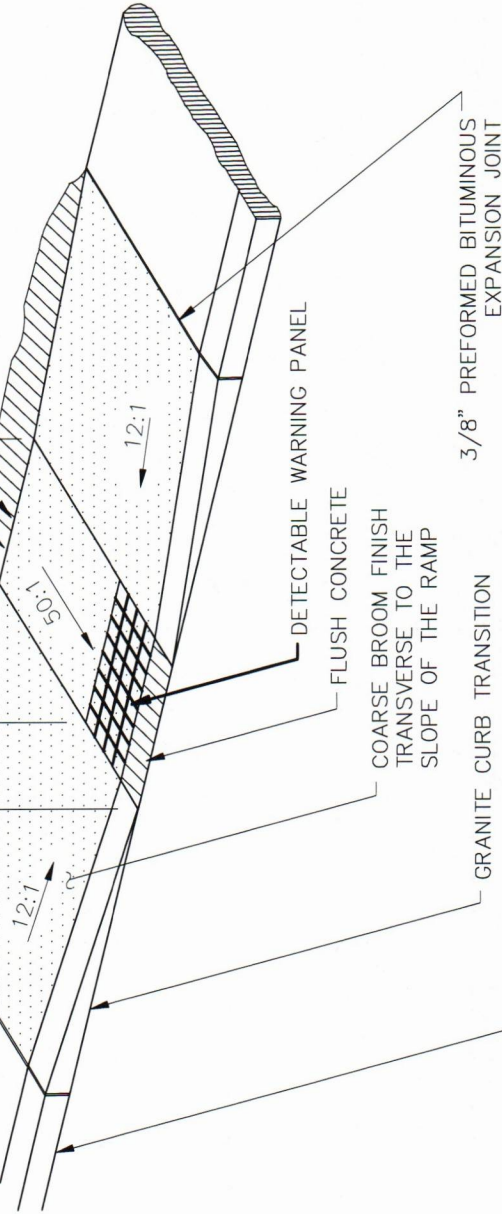
REINFORCED CONCRETE SIDEWALK SEE DETAIL

MEET EXISTING GRADE
BACK OF SIDEWALK

2'-0" MIN

2'-0" MIN

4'-0"



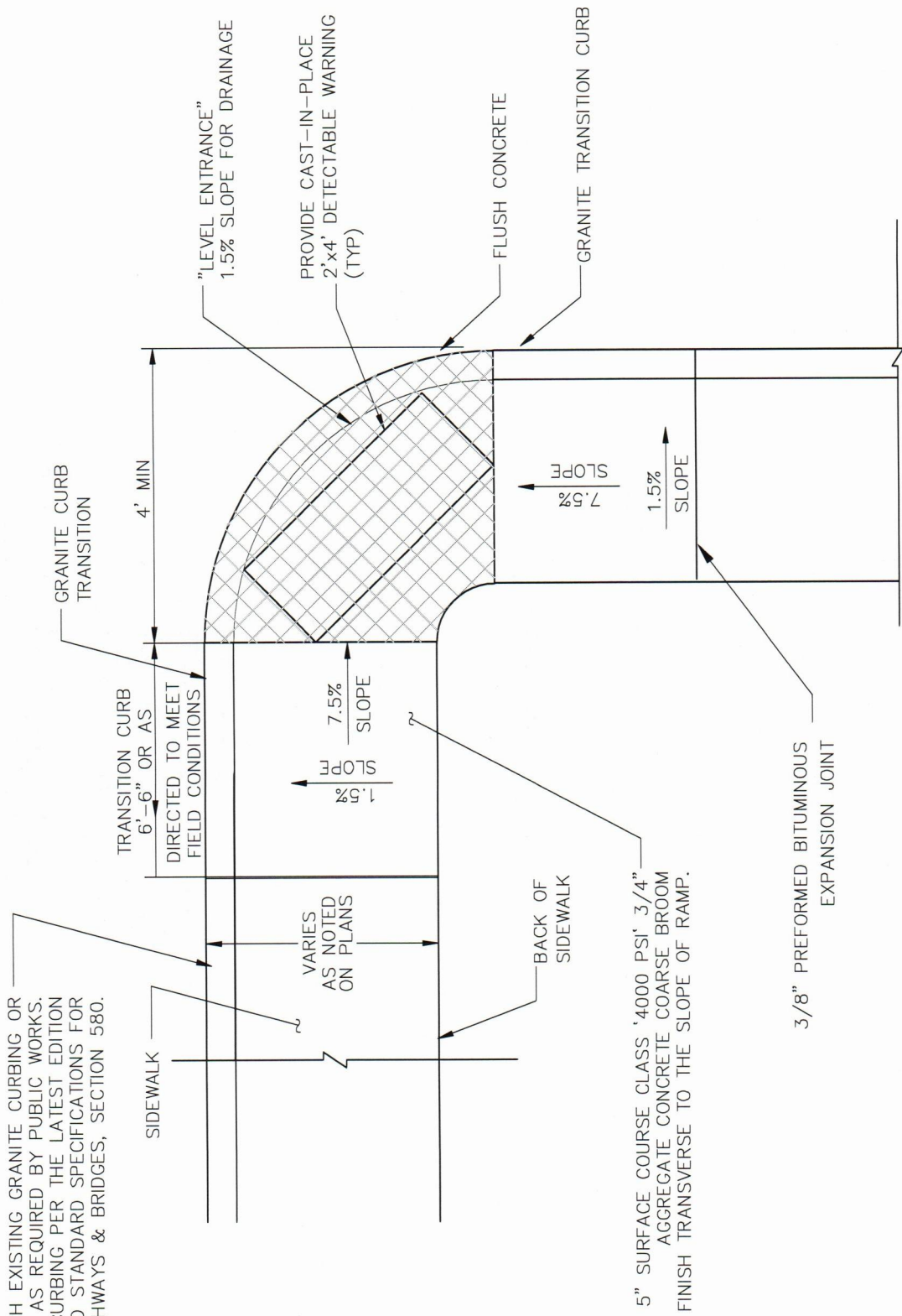
3/8" PREFORMED BITUMINOUS EXPANSION JOINT

GRANITE CURB TRANSITION

MATCH EXISTING GRANITE CURBING OR PROVIDE AS REQUIRED BY PUBLIC WORKS. INSTALL CURBING PER THE LATEST EDITION OF MHD STANDARD SPECIFICATIONS FOR HIGHWAYS & BRIDGES, SECTION 580.

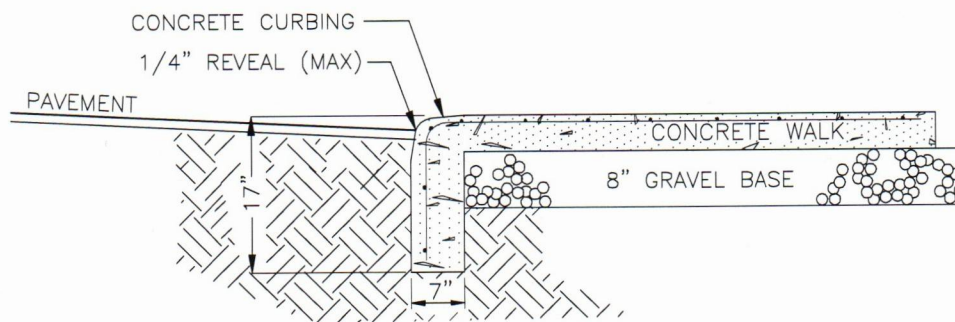
TYPICAL WHEELCHAIR RAMP
AT PERPENDICULAR CROSSINGS

NTS



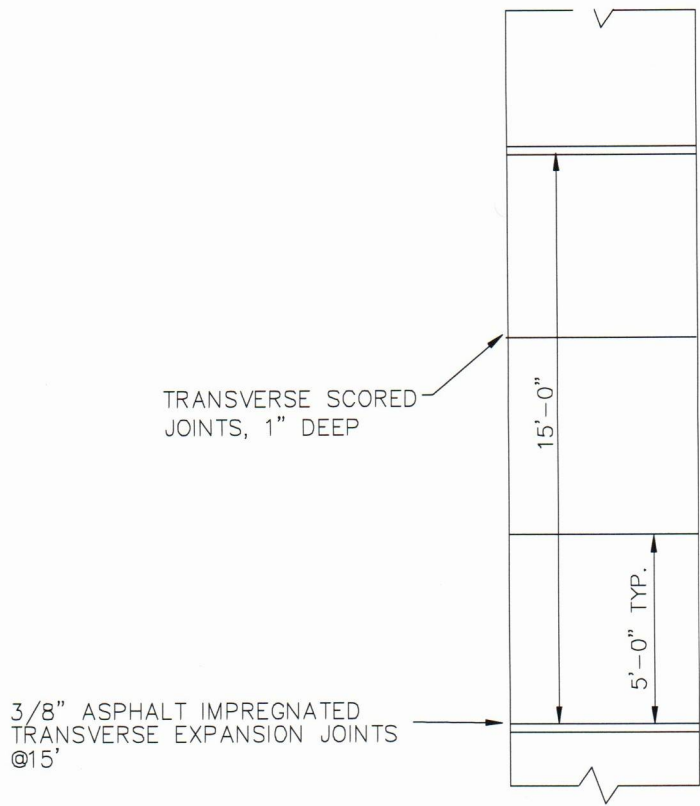
**TYPICAL WHEELCHAIR RAMP
FOR SINGLE TRAVEL DIRECTION**

NTS

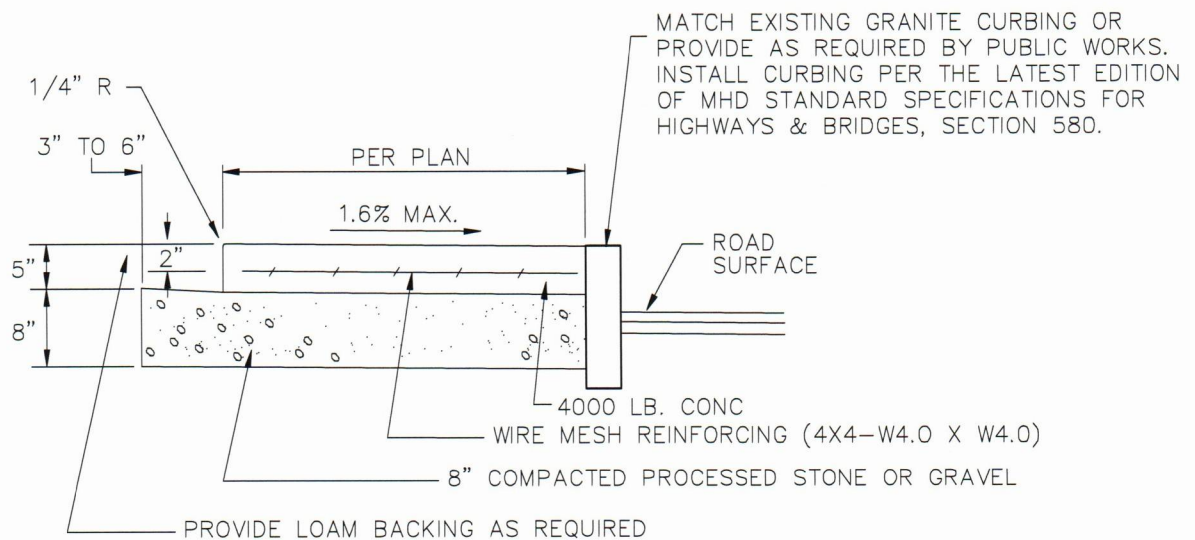


FLUSH CURB CROSS SECTION

CONCRETE SIDEWALK WITH
MONOLITHIC CONCRETE CURBING
NOT TO SCALE



PLAN



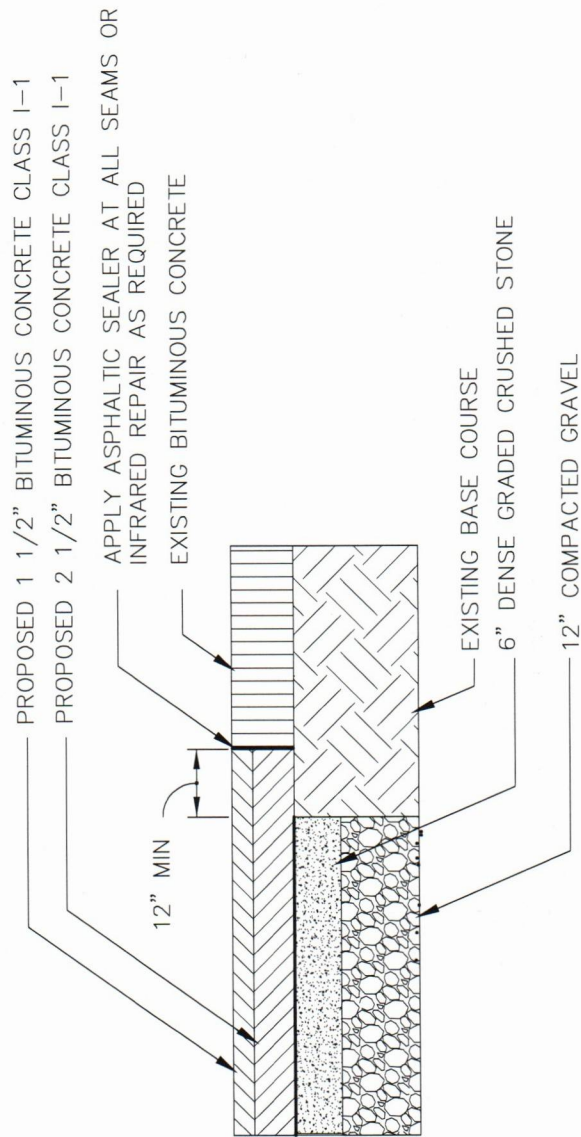
SECTION

STANDARD CONCRETE SIDEWALK DETAIL

NTS

NOTE:

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, CONTRACTOR TO PROVIDE A 2'x2' SQUARE BOX OUT AROUND ALL UTILITY POLES & HYDRANTS. THE 2'x2' SQUARE SHALL BE POURED INDEPENDENTLY OF THE ADJACENT SIDEWALK WITH 3/8" ASPHALT IMPREGNATED EXPANSION JOINT ON ALL SIDES ABUTTING CONCRETE.



ROADWAY PAVEMENT REPAIR DETAIL

NTS

NOTE:

1. ALL PAVEMENT SHALL CONFORM TO PAVEMENT MASSDOT SPECIFICATIONS
2. ALL SEAMS SHALL BE SAWCUT AND STRAIGHT.